

Civil and Tunnel Construction Contract Information Session

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Transbay Joint Powers Authority

- In 1999, San Francisco voters approved Proposition H making it City law to extend the northern terminus of Caltrain from south of downtown to a new or rebuilt regional transit station at the site of what was then the existing Transbay Terminal.
- The Transbay Joint Powers Authority (TJPA) was created under state law in April 2001. The TJPA has primary jurisdiction with respect to all matters concerning the financing, design, construction, and operation of the Transbay Program.
- Joint Powers Authority Member Agencies
 - **City & County of San Francisco**
 - Office of the Mayor
 - San Francisco Municipal Transportation Agency
 - San Francisco Board of Supervisors
 - **Alameda-Contra Costa Transit District**
 - **Peninsula Corridor Joint Powers Board (Caltrain)**
 - City & County of San Francisco
 - San Mateo County Transit District
 - Santa Clara Valley Transportation Authority
 - **California High-Speed Rail Authority**
 - **California Department of Transportation (Caltrans)**



Transbay Program



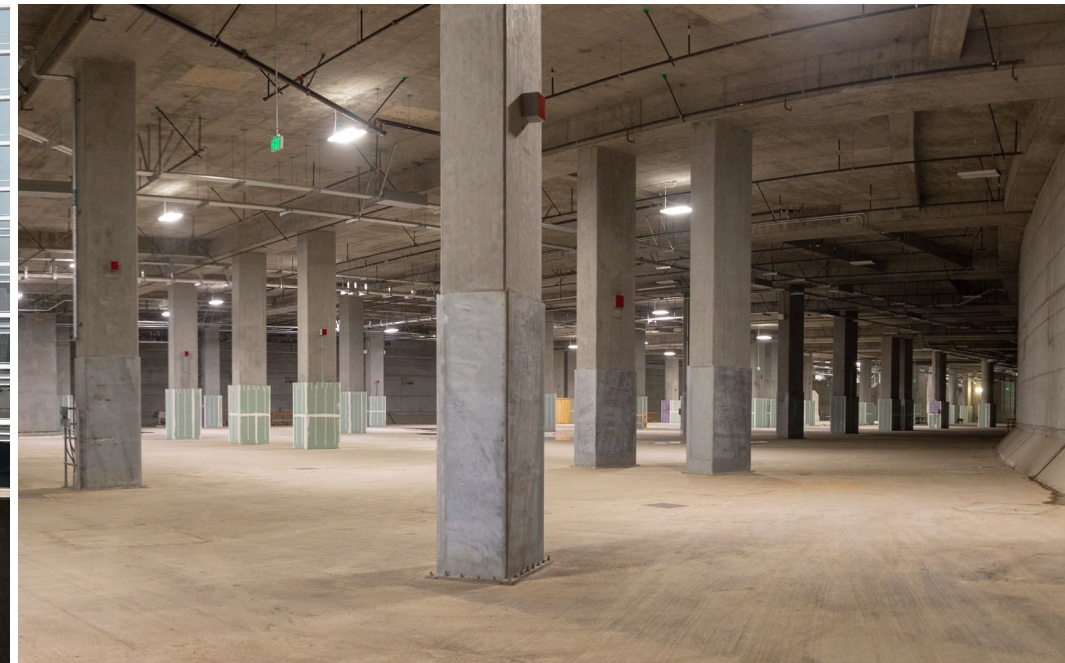
Before



After

- Replace former Transbay Terminal with a multimodal Salesforce Transit Center
- Create a transit-oriented neighborhood featuring housing (35% affordable), open space, offices and shops
- Extend Caltrain and California High-Speed rail service into downtown San Francisco through The Portal.

Phase 1: Salesforce Transit Center

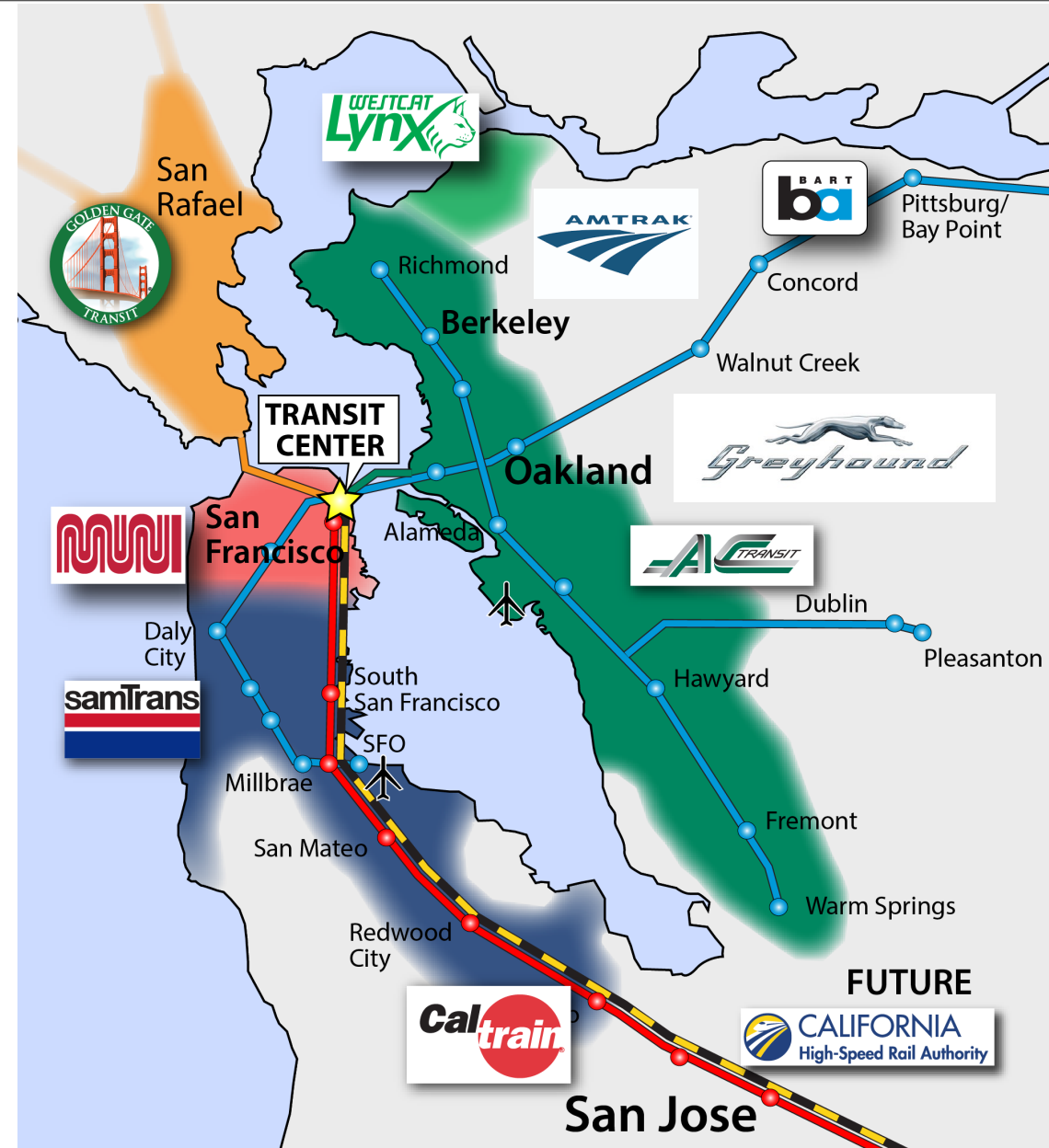


- The multimodal facility in downtown San Francisco is operational, providing transit, retail and wellness services.
- Four-blocks consisting of six-levels: four-levels open to the public while two-levels awaiting rail connections.
- Completed two-level below-grade train box
- Funded with a \$400 million *American Reinvestment and Recovery Act* grant
- Ready for station fit-out and rail service

Regional Transportation Hub

The Salesforce Transit Center connects nine regional and local transit systems, providing jobs, housing and access from the East Bay to the Peninsula and South Bay.

Phase 2 – The Portal – when complete will add direct connections to Caltrain's 77-mile system from the Peninsula and California high-speed rail to Southern California.



The Portal Benefits



- Allow seamless transfers of up to 90,000 daily riders through 11 transit systems*
- Provide a one-seat ride into downtown San Francisco from Silicon Valley and through the state's high-speed rail system
- Divert over 19,000 auto trips daily (in its first year of full operation)
- Provide equity priority communities, located within 1,000 feet of the two stations, access to an integrated transit system
- Improve air quality and reduces vehicle miles traveled (9.8 million metric tons of CO₂ reduced over 50 years)
- Create thousands of jobs, including through Small and Disadvantaged Business Enterprises and veterans' programs
- Enhance riders' access to employment, housing and services within San Francisco, along the Peninsula corridor and throughout the greater Bay Area

*Average of Caltrain, CHSRA, and TJPA ridership estimates

Full Name

Procurement Type

Utility Relocation

Design-bid-build (DBB)

4th and King Yard Preparation
Package A: Site Clearing

DBB or
Construction Manager/General Contractor (CMGC)

4th and King Yard Preparation
Package B: Track and Systems

DBB or CMGC

Building Demolition

DBB

Civil and Tunnel

Progressive Design-Build (PDB)

Track and Rail Systems

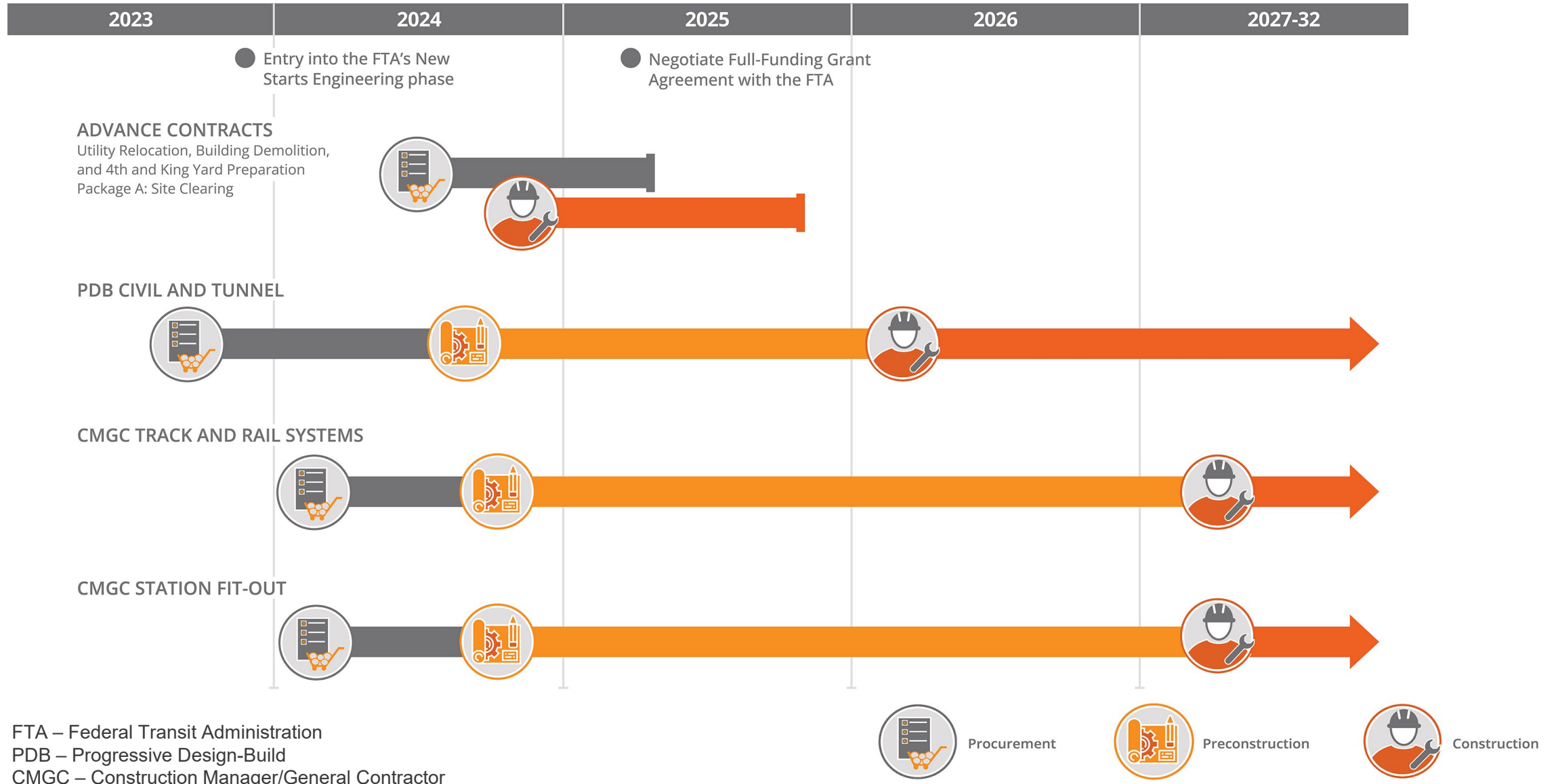
CMGC

Transit Center Station Fit-out

CMGC

Project Delivery

*Dates are subject to change and dependent on funding availability



FTA – Federal Transit Administration
PDB – Progressive Design-Build
CMGC – Construction Manager/General Contractor

Civil and Tunnel Procurement Timeline

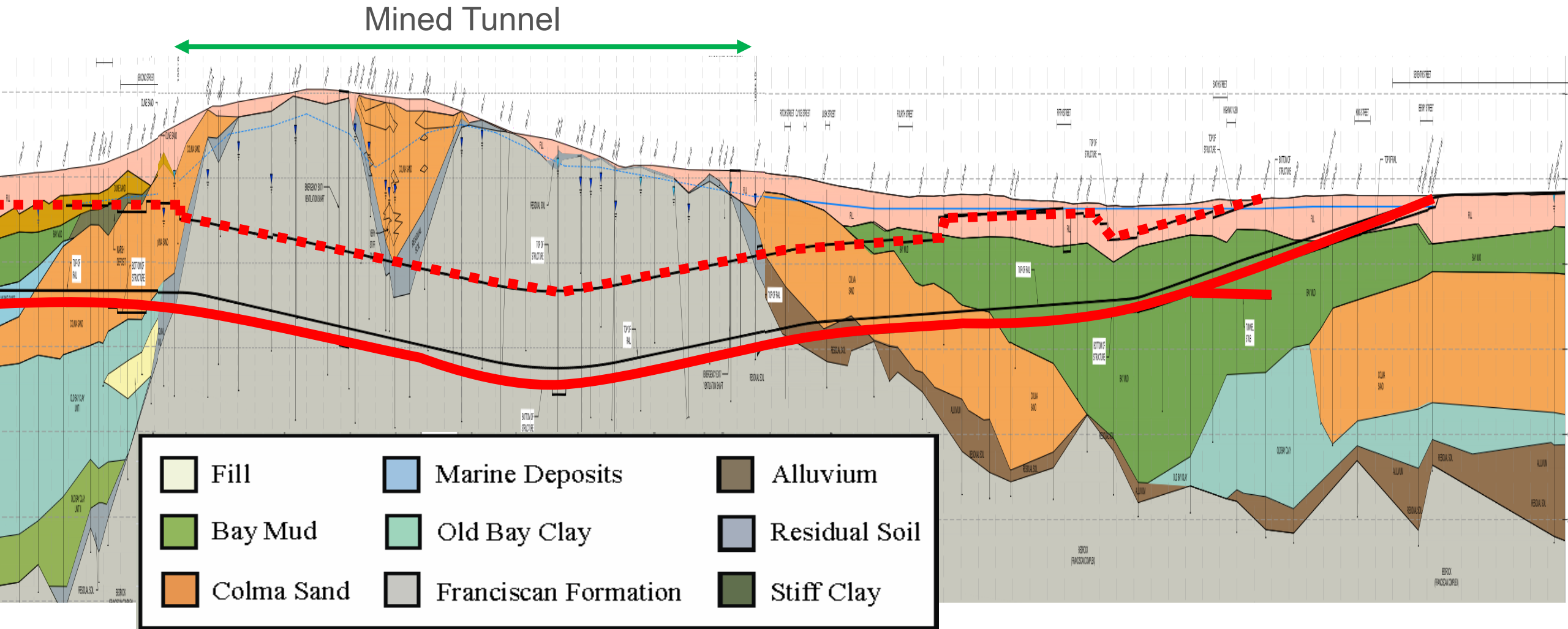


*Dates are subject to change and dependent on funding availability

Alignment Map



Vertical Alignment and Soil Profile



----- Structure Crown

———— Top of Rail

———— Structure Invert

Four Main Elements

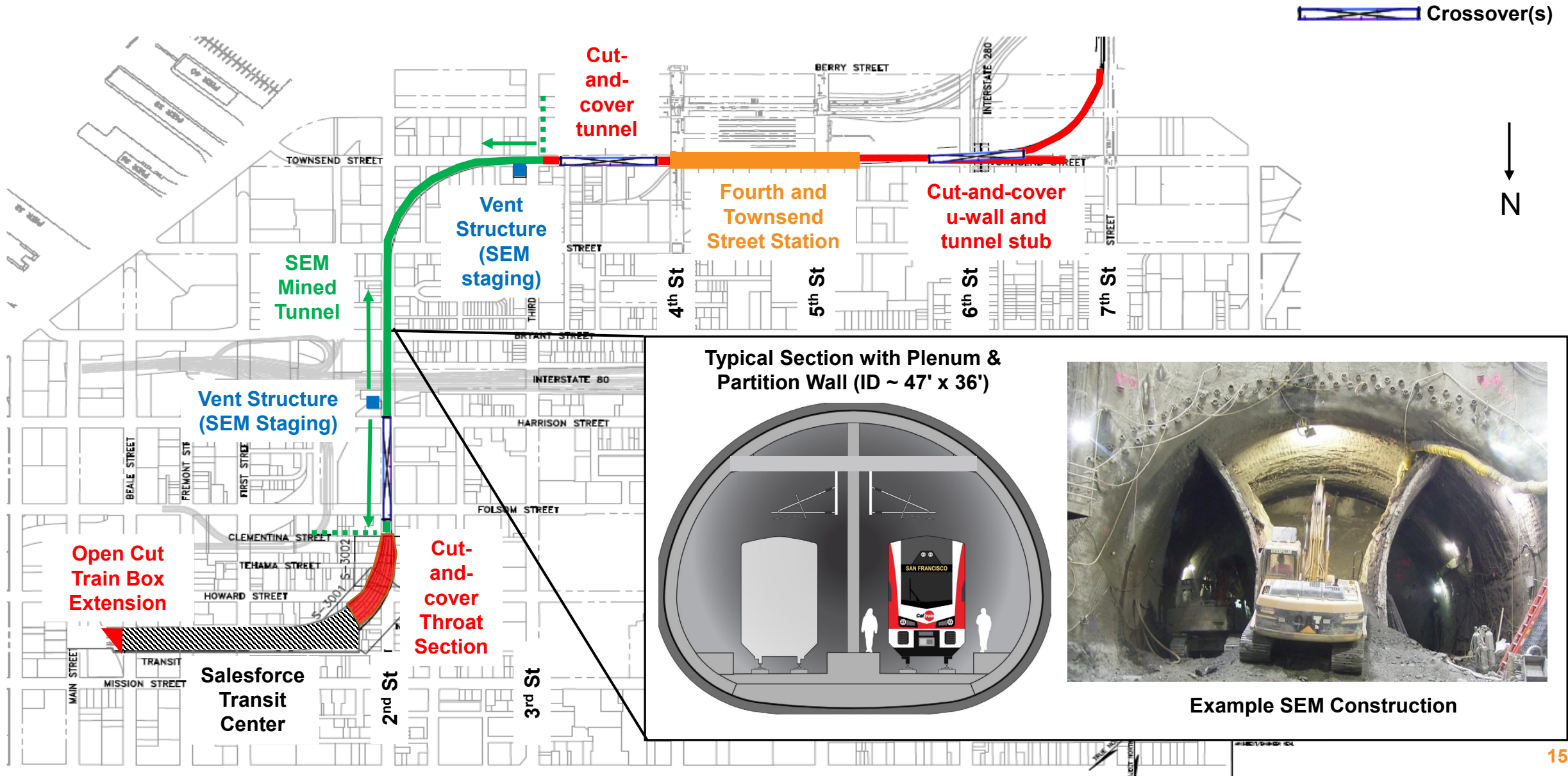
1. Mined tunnel
2. Cut-and-cover sections
3. Ventilation structures
4. Fourth and Townsend Street Station fit-out *

*TJPA option to remove this portion from the PDB scope

1. Mined Tunnel and Shafts

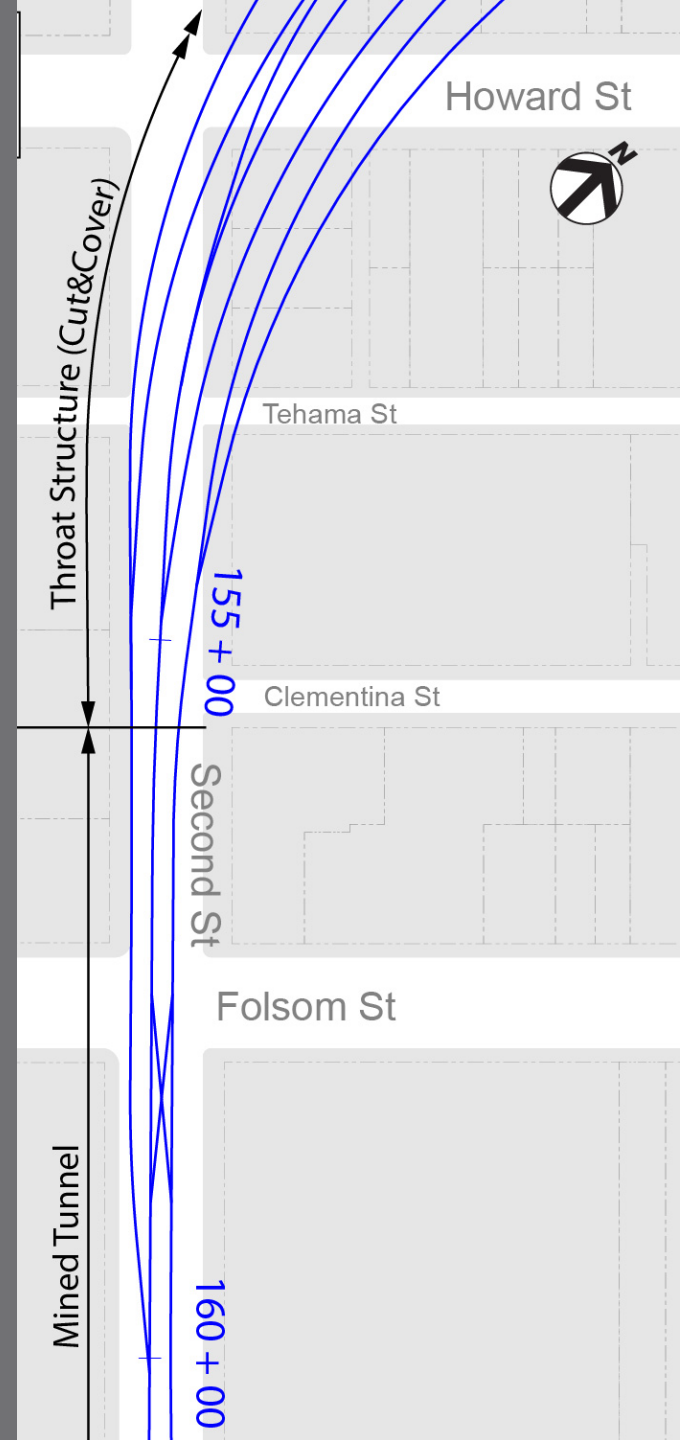
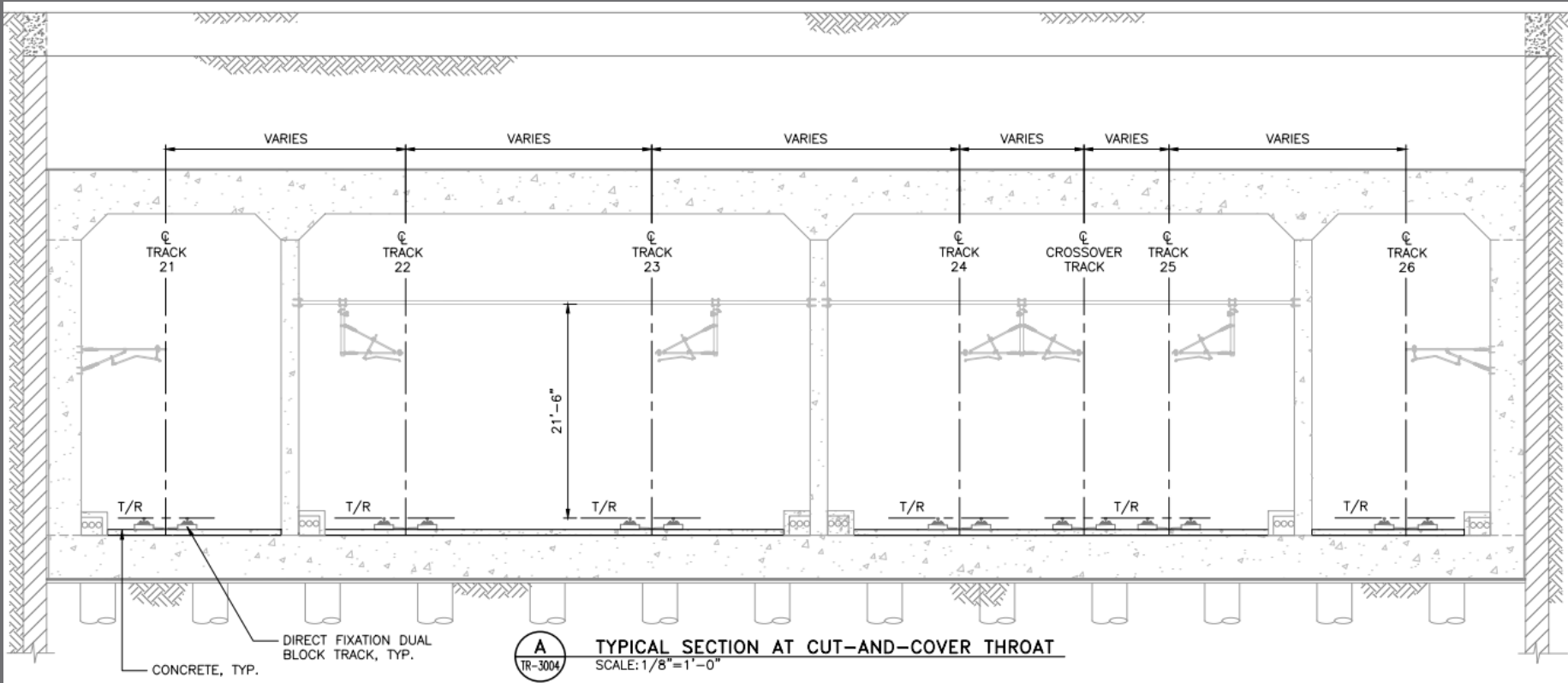
- ~0.65-mile, two-track mined tunnel
- Two shafts located at future ventilation structure sites
- Excavation
- Initial support
- Waterproofing
- Final lining, partition walls, plenum slab
- Secondary concrete pours to include walkways
- Drainage

Single Bore Sequential Excavation Method (SEM)



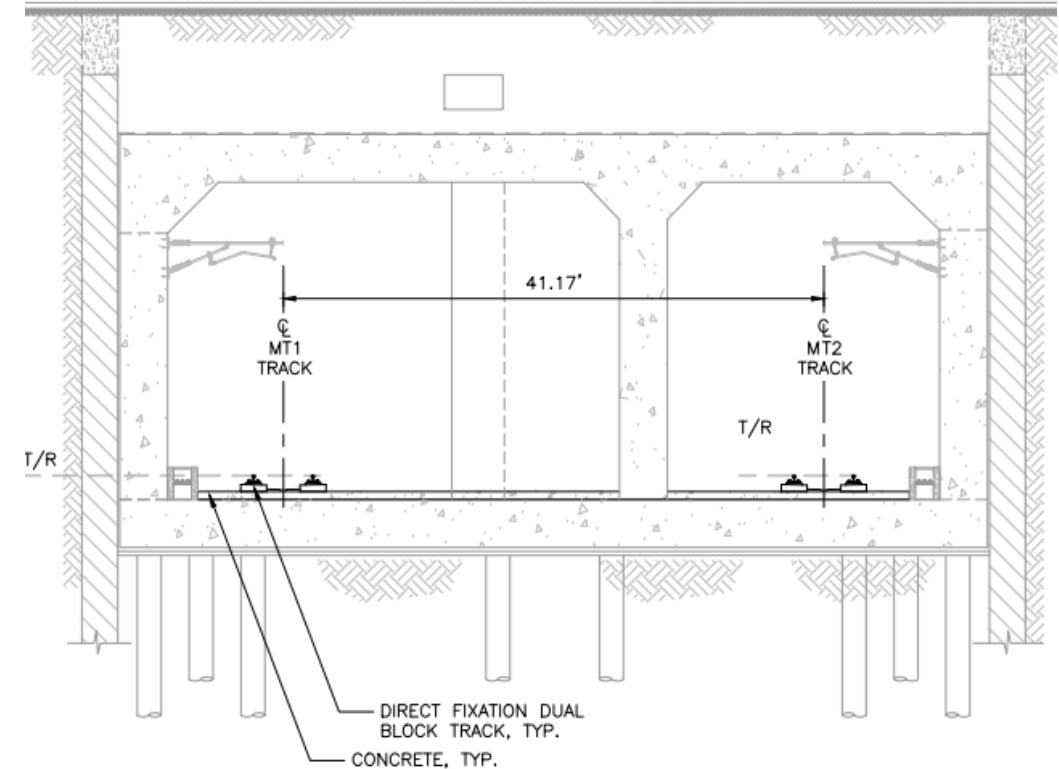
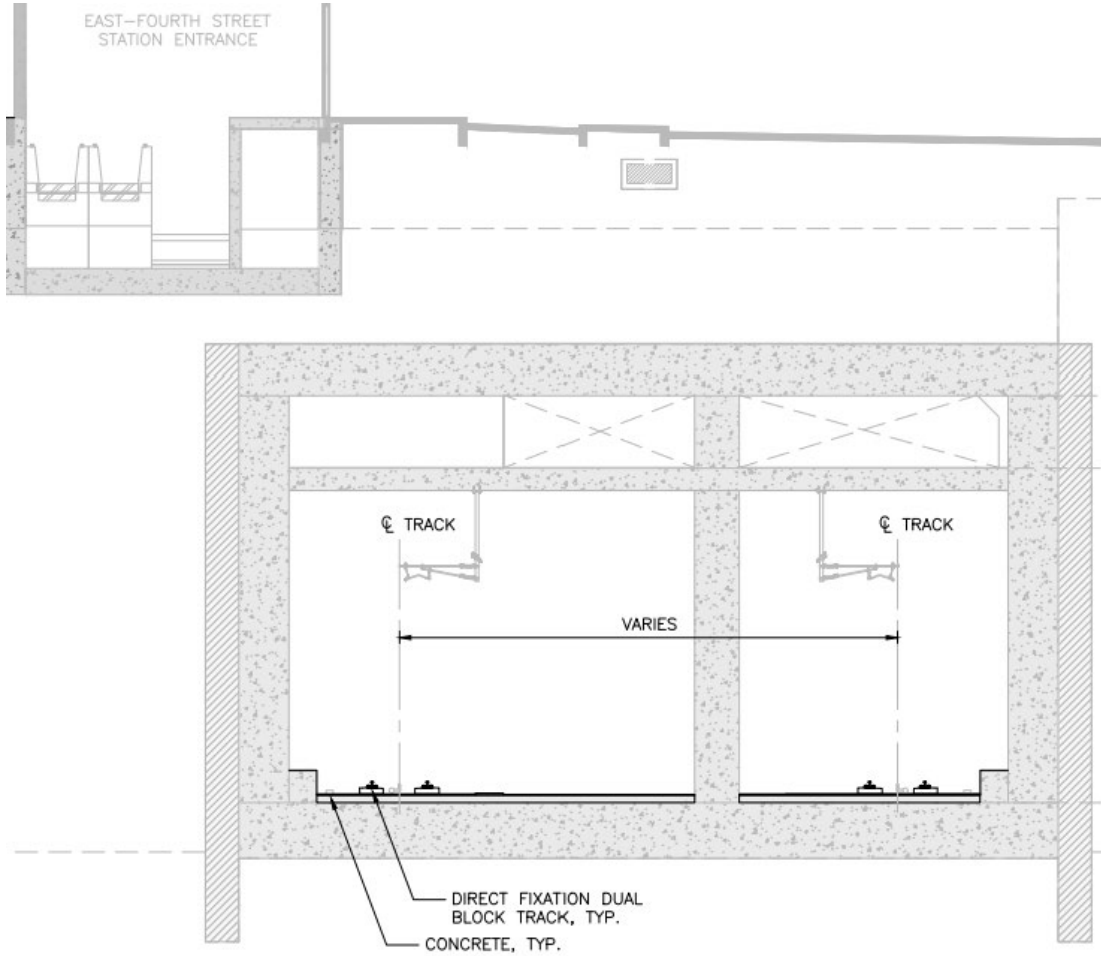
2. Cut-and-cover structures

- ~ 1 mile of cut-and-cover construction including:
 - Throat structure
 - Salesforce Transit Center trainbox extension (may be removed from scope)
 - Fourth and Townsend Street Station box & Townsend Street tunnel
 - U-wall & tunnel stub box
- Support of excavation
- Final concrete structures
- Utility support, permanent placement, and diversion of Sixth Street sewer
- Roadway reinstatement



Throat Structure

Cut-and-Cover Fourth and Townsend Street Station Box & Townsend Street & Townsend Tunnel



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TR-3013

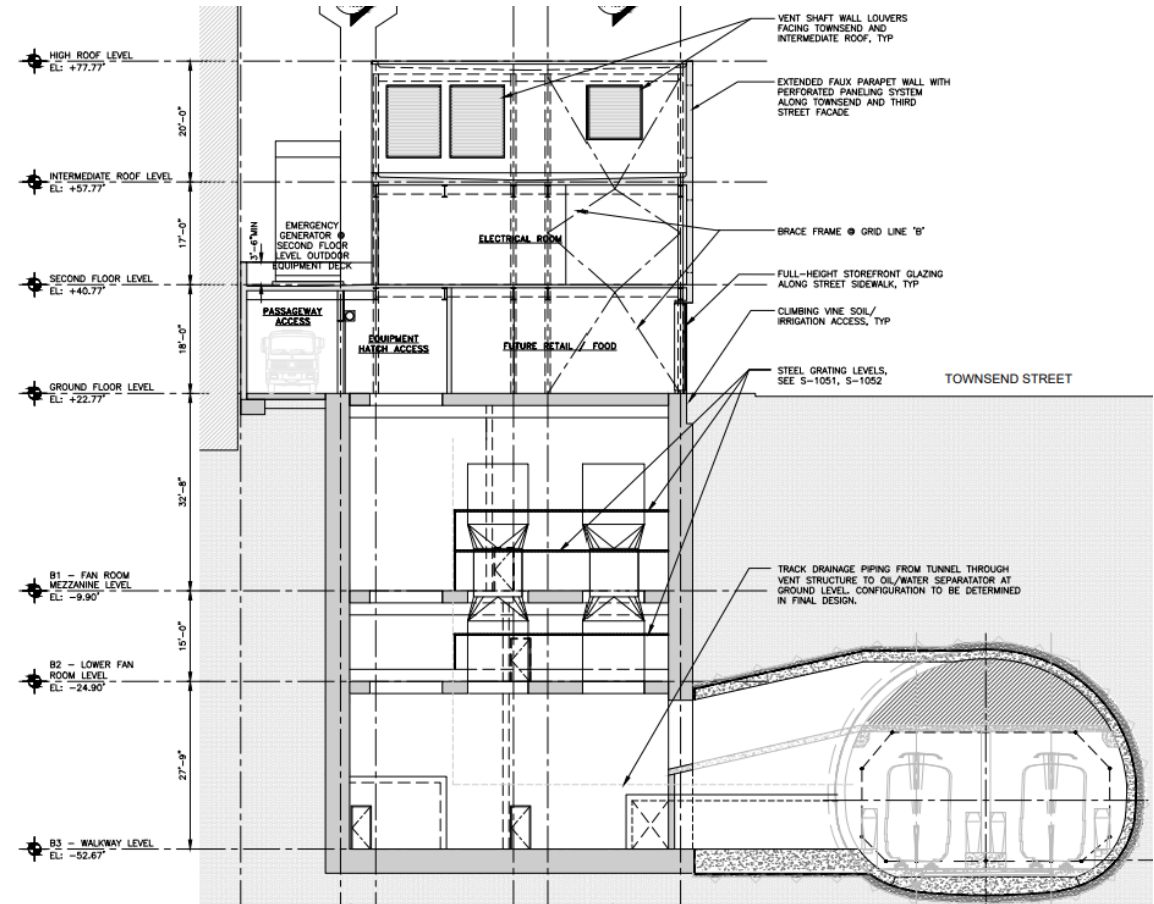
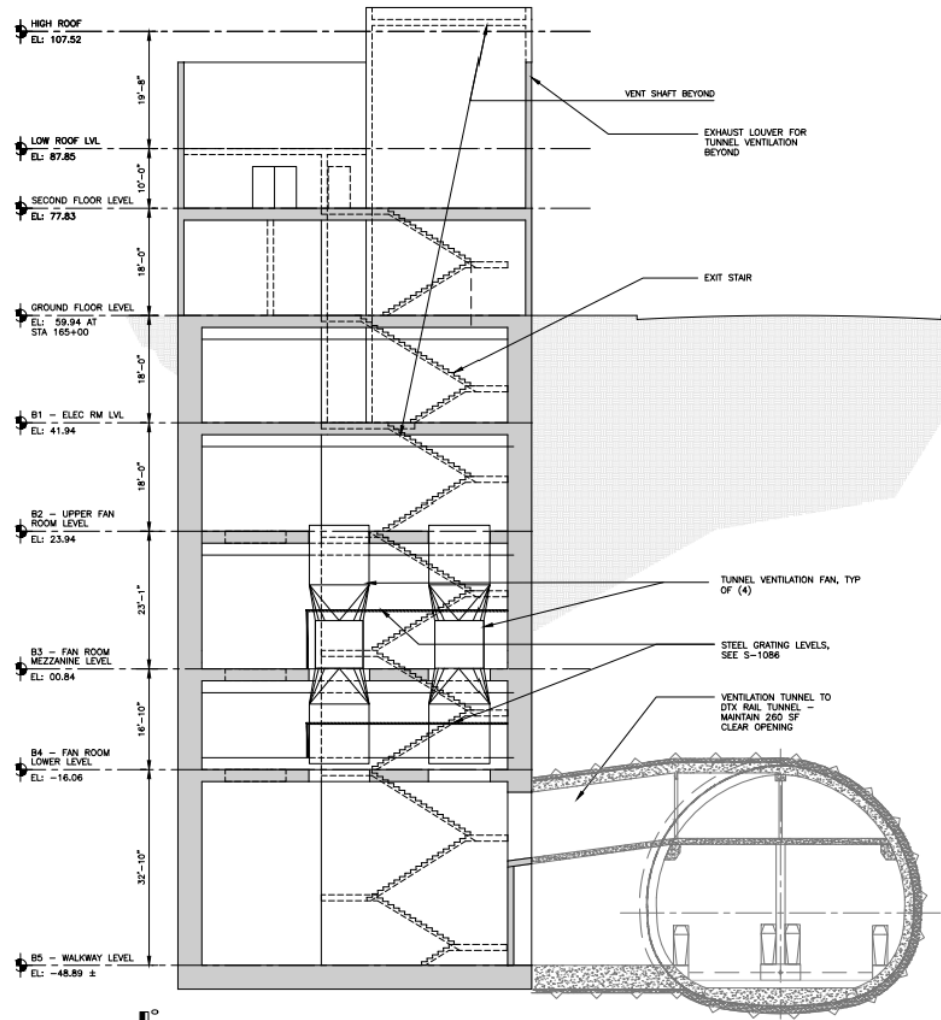
TYPICAL SECTION AT
CUT-AND-COVER WEST
SCALE: 1/8"=1'-0"

3. Ventilation and emergency egress shafts, structures, and fit-out

- Final concrete structures
- Above-ground structures
- Building mechanical and electrical systems, excluding rail systems
- Building specialties, fittings, and accessories

Ventilation & Emergency Egress Structures

Vent Structure at Second & Harrison



Vent Structure at Third & Townsend

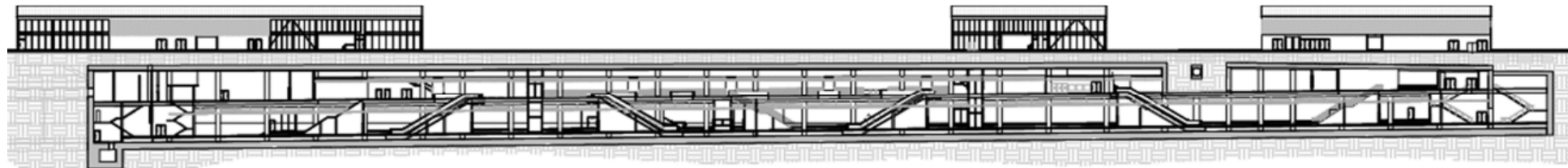
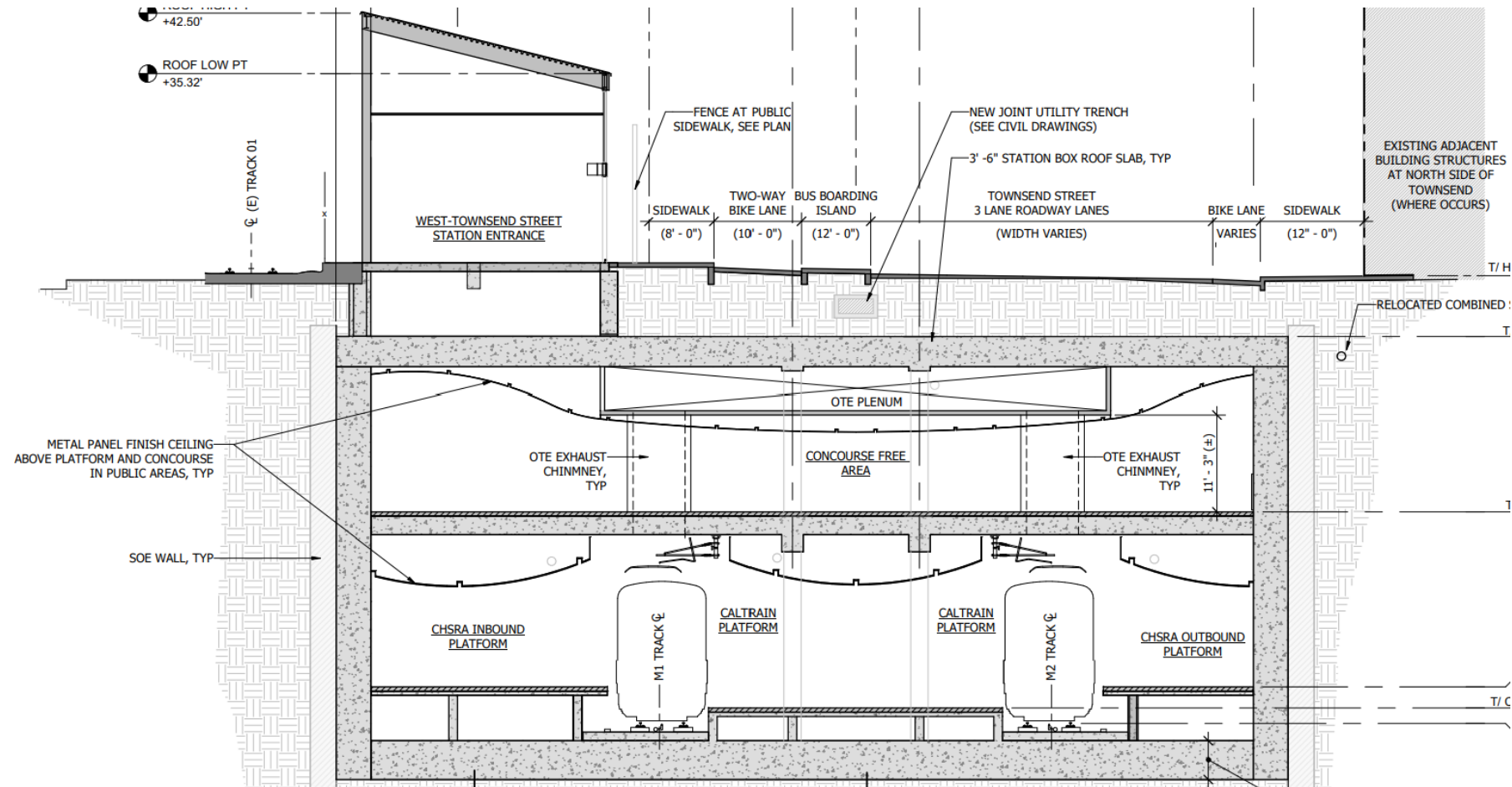
* Additional Vent Shafts at the Transit Center and Fourth and Townsend St. Station as required by NFPA 130 code

4. Fourth and Townsend Street Station Fit-out *

- Above-ground structures
- Building mechanical and electrical systems, excluding rail systems
- Vertical conveyances
- Station platforms
- Building specialties, fittings, and accessories
- Artwork
- Landscaping

*TJPA option to remove this portion from the PDB scope

Fourth and Townsend Street Station Fit Out



RFQ will ask for:

- Qualifications from similar projects
- Qualifications for key personnel
- Lessons learned
- Small / Disadvantaged Business Enterprise (SBE/DBE) experience

- SBE program adopted in 2009
- Enforceable SBE goals are placed on contracts as appropriate
- Meeting the goal or demonstrating good faith efforts to do so is a condition of contract award
- Programwide SBE goal of 20.5% in the aggregate for all contracts, regardless of funding source, over three fiscal years (FFY23-25)
- Specific SBE goal for PDB contract will be specified in RFQ

- DBE Program adopted in 2006
- Anticipated DBE participation level of 17% in the aggregate for FTA-assisted contracts spanning over three fiscal years (FFY 23-25)
- PDB contract is an FTA-assisted contract
- Specific DBE goal for PDB contract may be specified in RFQ

- TJPA accepts certifications from the following programs:
 - State of California Department of General Services, Procurement Division, Office of Small Business and Disabled Veteran Business Enterprise Services
 - City and County of San Francisco Local / Minority/Woman-Owned Business Enterprise as certified by the Contract Monitoring Division of the Office of the City Administrator
 - Any state's Unified Certification Program, including the California Unified Certification Program



Thank you



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